

THE BLVD

MUHAMMAD ALI • ARTS, CULTURE, & INNOVATION DISTRICT

THE WORLD'S **GREATEST** MILE.
DESIGN GUIDELINES

THE BLVD

MUHAMMAD ALI • ARTS, CULTURE, & INNOVATION CORRIDOR



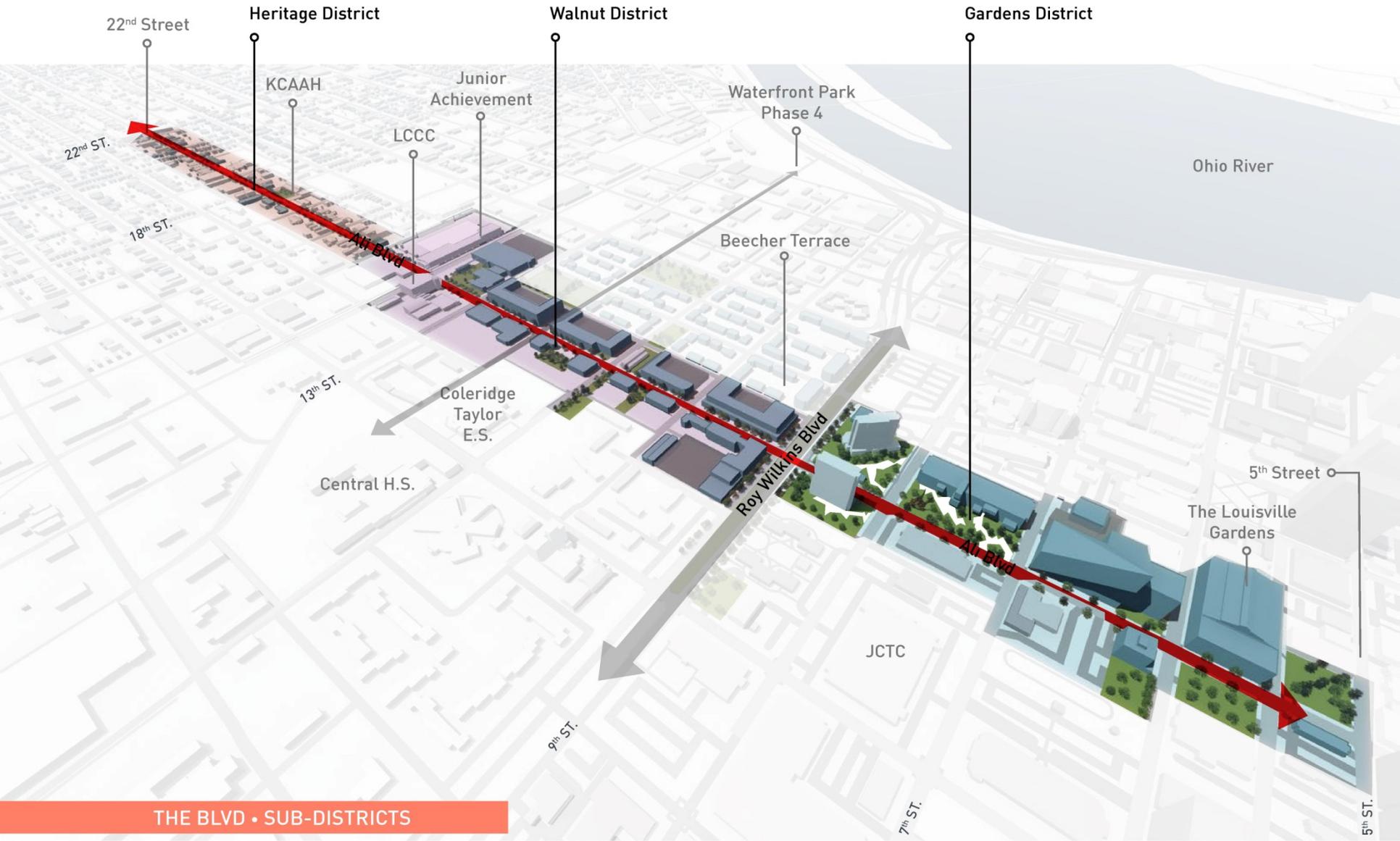
WALNUT STREET GROUP



The Louisville Central Community Center (LCCC) had a study done by the AIA Communities by Design an arm of the American Institute of Architects (AIA) to identify ways to revitalize the Old Walnut Street, currently called Muhammad Ali Blvd. which was once a thriving business and entertainment district for African Americans. The objective of the study was to use a community engagement process to uncover alternatives for using urban design, mobility, and green infrastructure to leverage the rich history of the area for sustainable community development. The process asks designers, planners, and community partners to consider how two near-term investments can catalyze broader community revitalization in the context of the West of Ninth Vision Plan.







THE BLVD • SUB-DISTRICTS

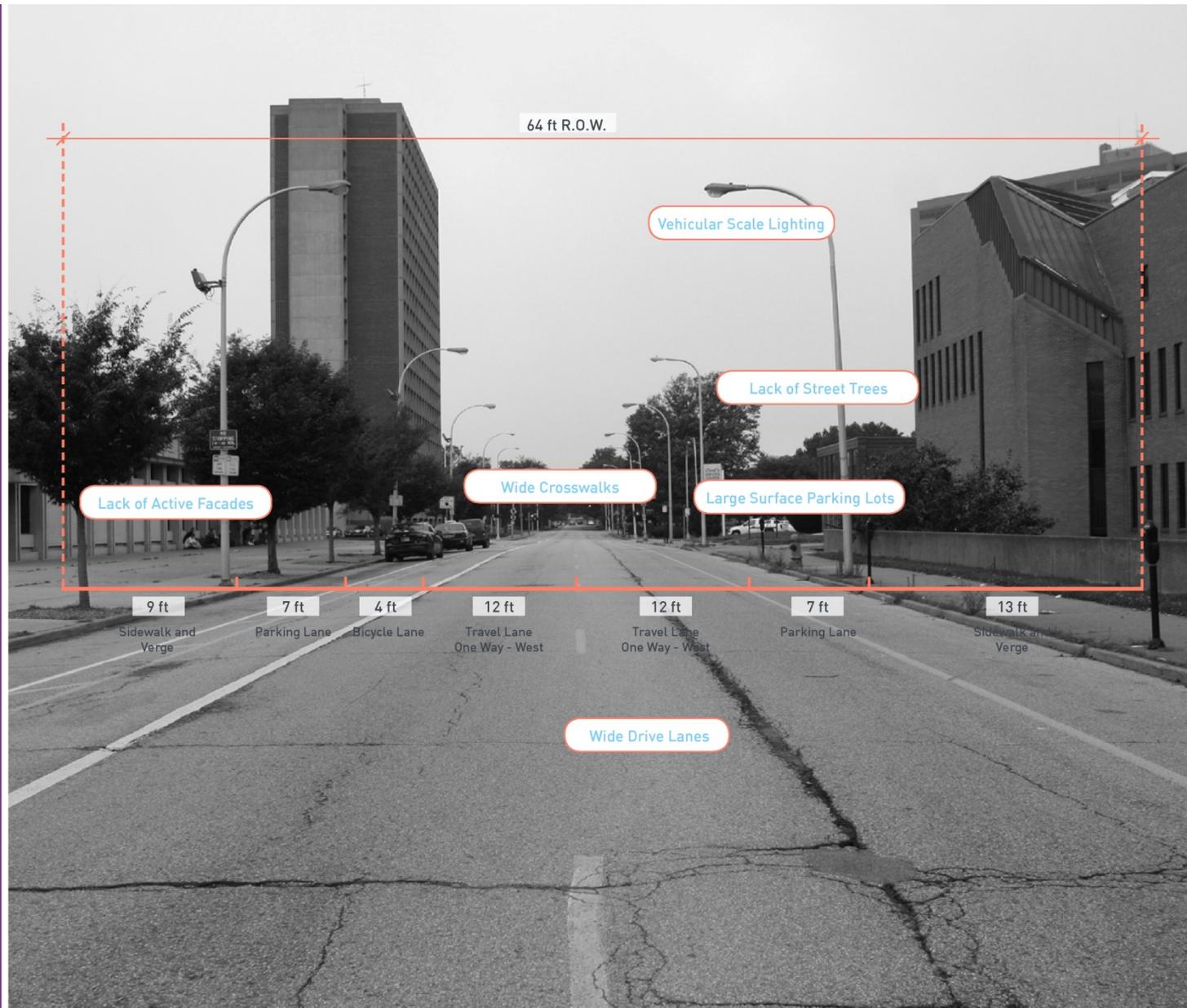
Gardens District

5th St - 9th St

EXISTING CONDITIONS

The Gardens District, named after the 6,000 seat arena built in 1905, represents an injection of vibrancy into an area that has become a void within the Central Business District.

- **Lack of Street Trees**
Street Trees create comfortable micro-climates, improve air and water quality, and frame the street.
- **Wide Drive Lanes**
Wide driving lanes encourage faster traffic making it less safe for pedestrians.
- **Vehicular Scale Lighting**
Vehicle scale lighting only focuses on lighting the street. Human lighting should be provided where people are present.
- **Large Surface Parking Lots**
Surface parking lots along the streetscape fragments the consistent street frontage.
- **Wide Crosswalks**
Wide crosswalks are less safe because it takes longer to cross the street and pedestrians are not as visible.
- **Lack of Active Facades**
Blank building facades and walls keep the buildings from engaging the streetscape.



Gardens District

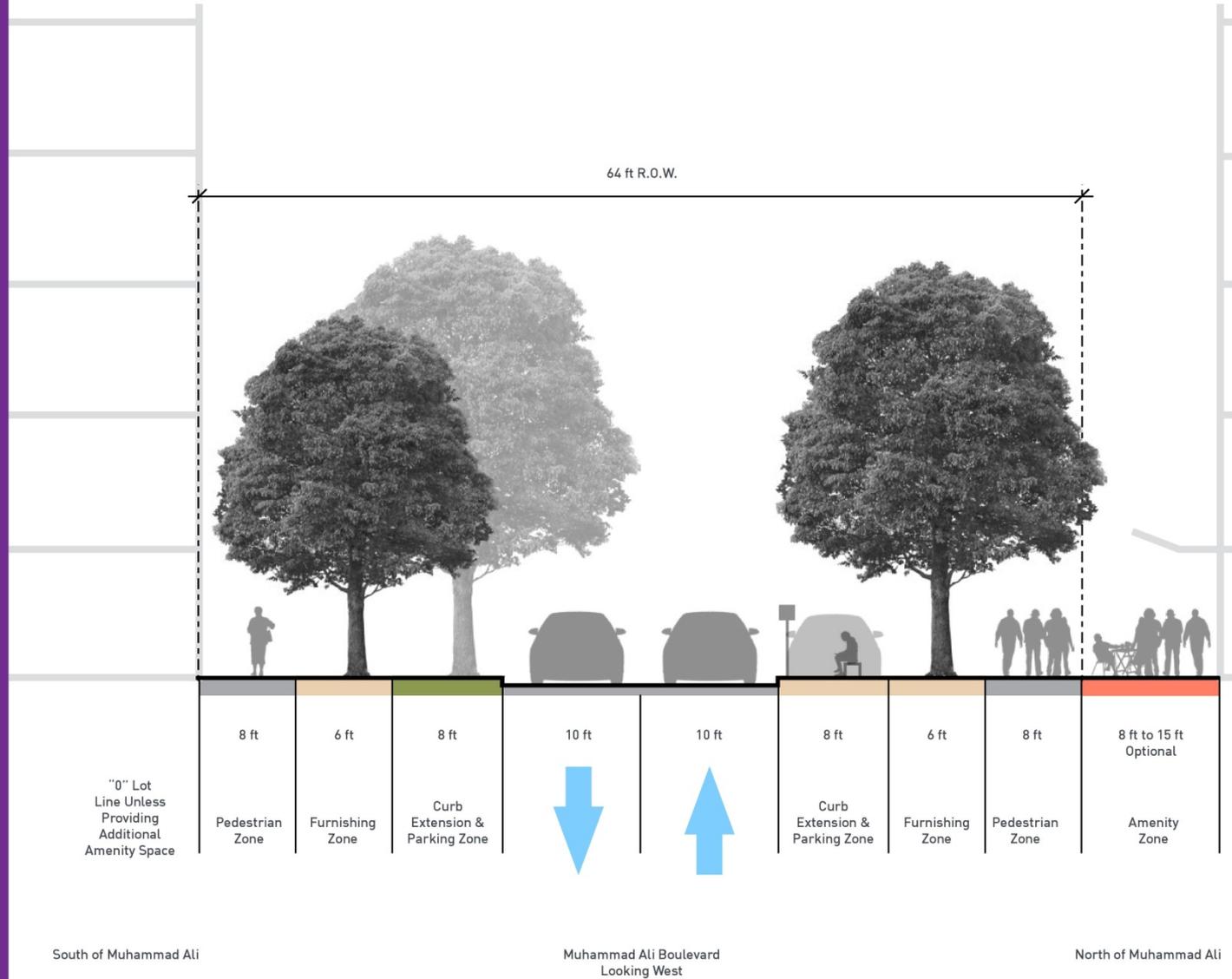
5th St - 9th St

PROPOSED SECTION - TYPICAL

The Gardens District should be an extension of the Central Business District with high pedestrian use and mixed uses. Generous sidewalks offer space for pedestrians and streetscape items like benches, landscaping, lighting, and bicycle parking. This district should provide a distinctive form that is different than the rest of the Arts, Culture, and Innovation District.

Buildings in this district will observe a 5 story minimum. Buildings above 8 stories shall offer additional pedestrian amenity areas at street level or within roof top terraces.

- Curb Extension / Parking Zone**
 Where parking is not designed, curb extensions offer safety to pedestrians, calms traffic, and provides extra space for amenities and public art. Curb extensions would typically occur at intersection corners and transit stops.
- Furnishings Zone**
 The furnishings zone is reserved for landscape and street furniture amenities, as well as, place making opportunities.
- Pedestrian Zone**
 The pedestrian zone is the sidewalk path where pedestrians travel. This path shall remain clear at all times, uninhibited from obstructions like street furniture, signage, and any plantings. The material for this zone is concrete throughout the Gardens District.
- Amenity Zone**
 The amenity zone is reserved for commercial and restaurant businesses to provide additional pedestrian amenity space.



THE GARDENS DISTRICT - EXISTING



THE GARDENS DISTRICT - PROPOSED

Looking West



"Street Wall"

Street Trees

Mixed-Use Infill

On Street Parking

Traffic Calming

Enhanced Landscape

Pedestrian Amenities

Interpretive Displays

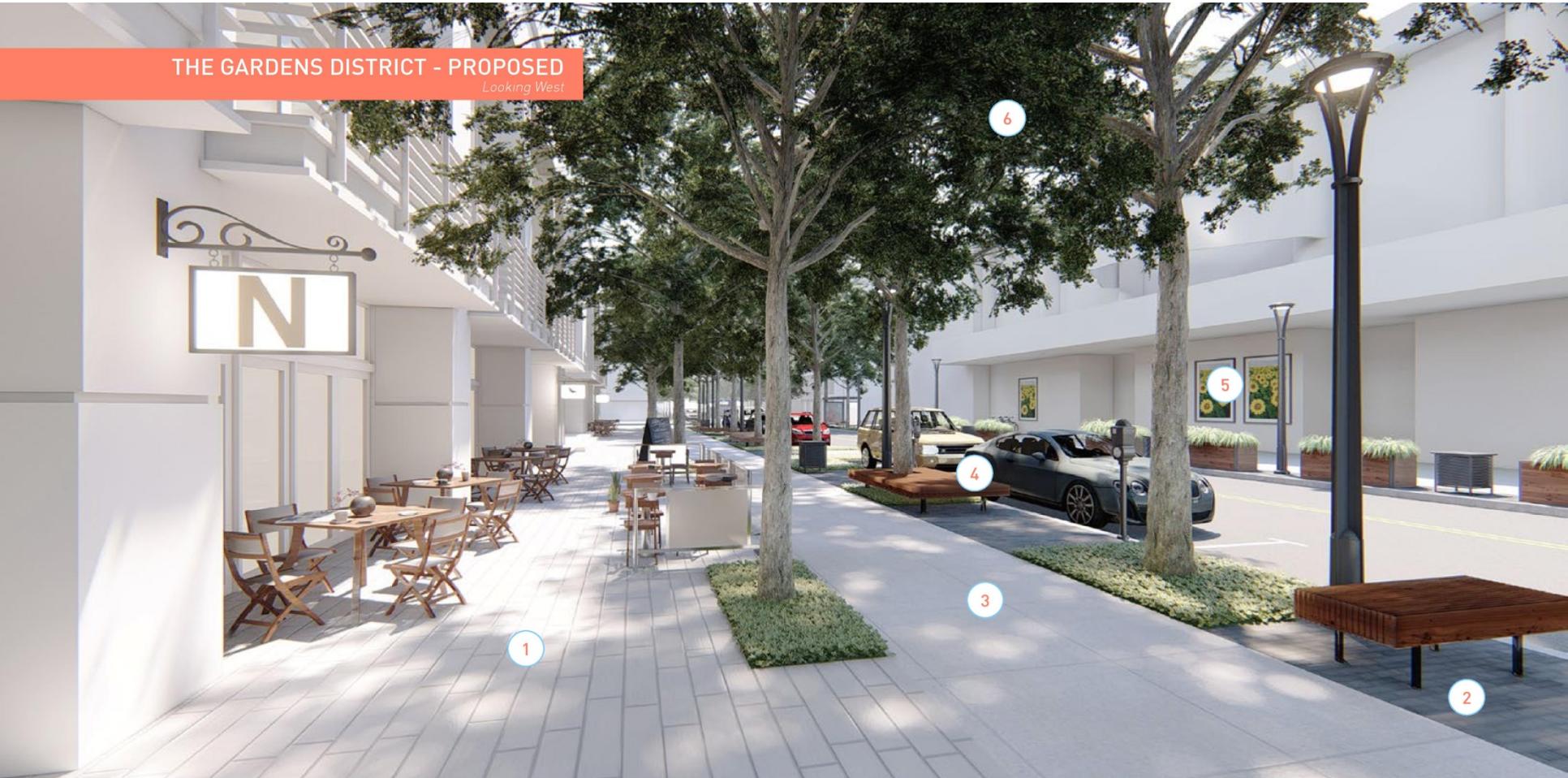
Wide Sidewalks

Former Louisville Gardens
Historic Adaptive Re-use

Public Art

THE GARDENS DISTRICT - PROPOSED

Looking West



Unique Details to the Garden District

- 1) Large, rectangular pavers in a running bond pattern, parallel to the street and located within amenity zones.
- 2) Medium, rectangular pavers in a running bond pattern, perpendicular to the street located within furnishing zone

- 3) Pedestrian zone is a concrete sidewalk path, except adjacent to The Gardens adaptive re-use project. This area shall be large pavers matching the amenity zone.
- 4) Dramatic, large platform and angular wood benches are located within the furnishing zone.

- 5) Public art opportunities are located at street corners/ curb extensions and mural walls.
- 6) Street trees in all districts shall be selected from the recommended urban tree list provided in the design guideline portion of this document. Similar tree species should be planted in succession in lines of eight to ten

trees to help create a movement along the corridor. Tree species on the recommended urban tree list will vary in mature size and should be determined based on land-uses adjacent to the plantings.

THE GARDENS DISTRICT - PROPOSED

Looking West



River City Bank (Mammoth Life)

THE GARDENS DISTRICT - PROPOSED

Looking West

Former Louisville Gardens
Historic Adaptive Re-use



Walnut District

9th St - 14th St

EXISTING CONDITIONS

The Walnut District includes planned improvements made possible through the \$30M Choice Neighborhoods Implementation Grant at Beecher Terrace, that should help attract opportunities such as adaptive reuse of the former Porter Paints Factory, new connections and programming through Central HS and Coleridge Taylor ES and potential redevelopment of the City View Park Apartments.

- **Zero Pedestrian Amenities**

The lack of pedestrian amenities like benches make this sidewalk uncomfortable for the pedestrian.

- **Lack of Consistent Street Frontage**

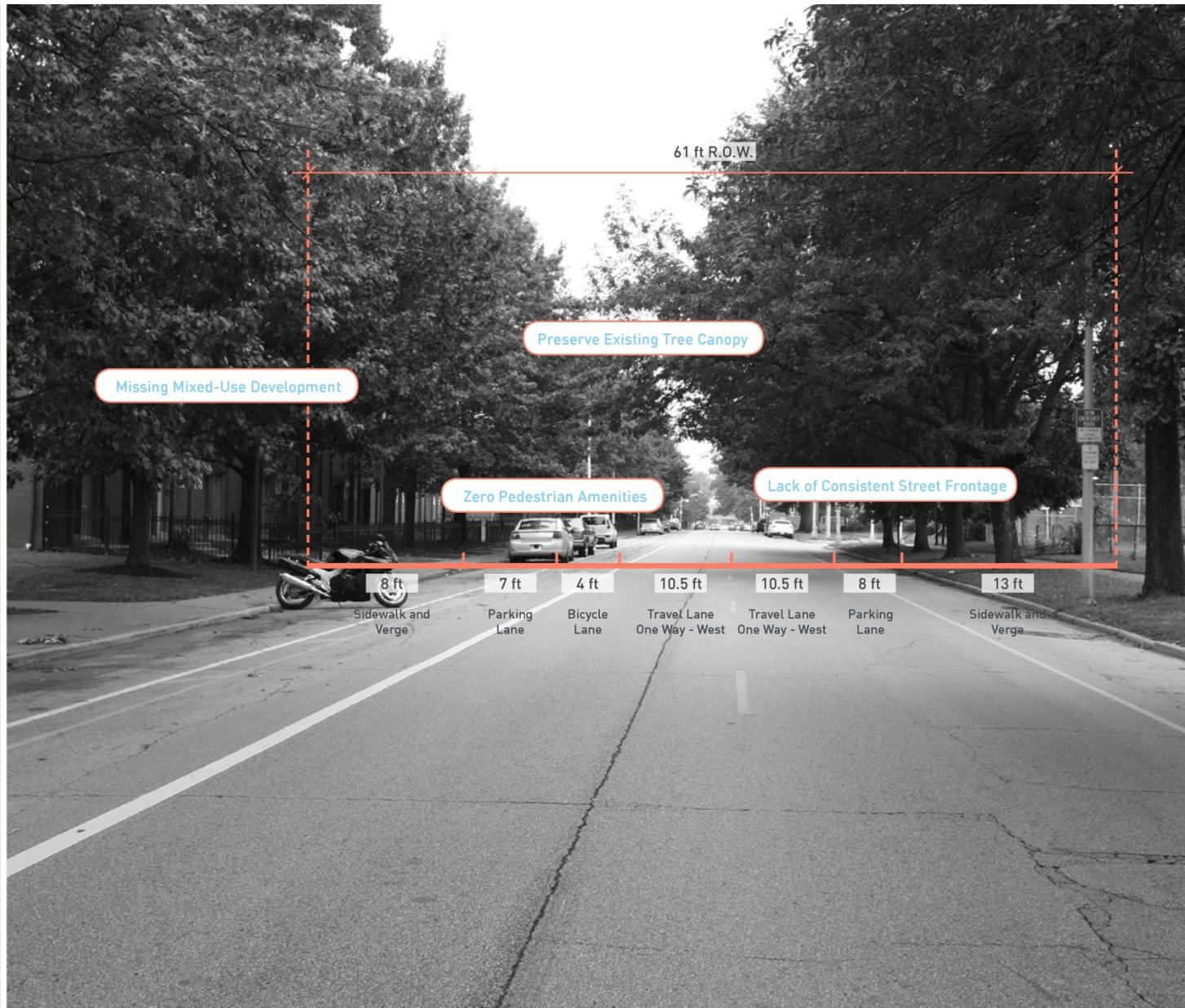
The street frontage is inconsistent along this corridor which contributes to the large building setbacks.

- **Preserve Existing Tree Canopy**

The mature tree canopy is healthy for this neighborhood and offers many benefits like air and water quality, comfortability, and safety-buffer.

- **Missing Mixed-Use Development**

Mixed-Use development offering goods and services for community members is necessary for thriving neighborhoods.



Walnut District

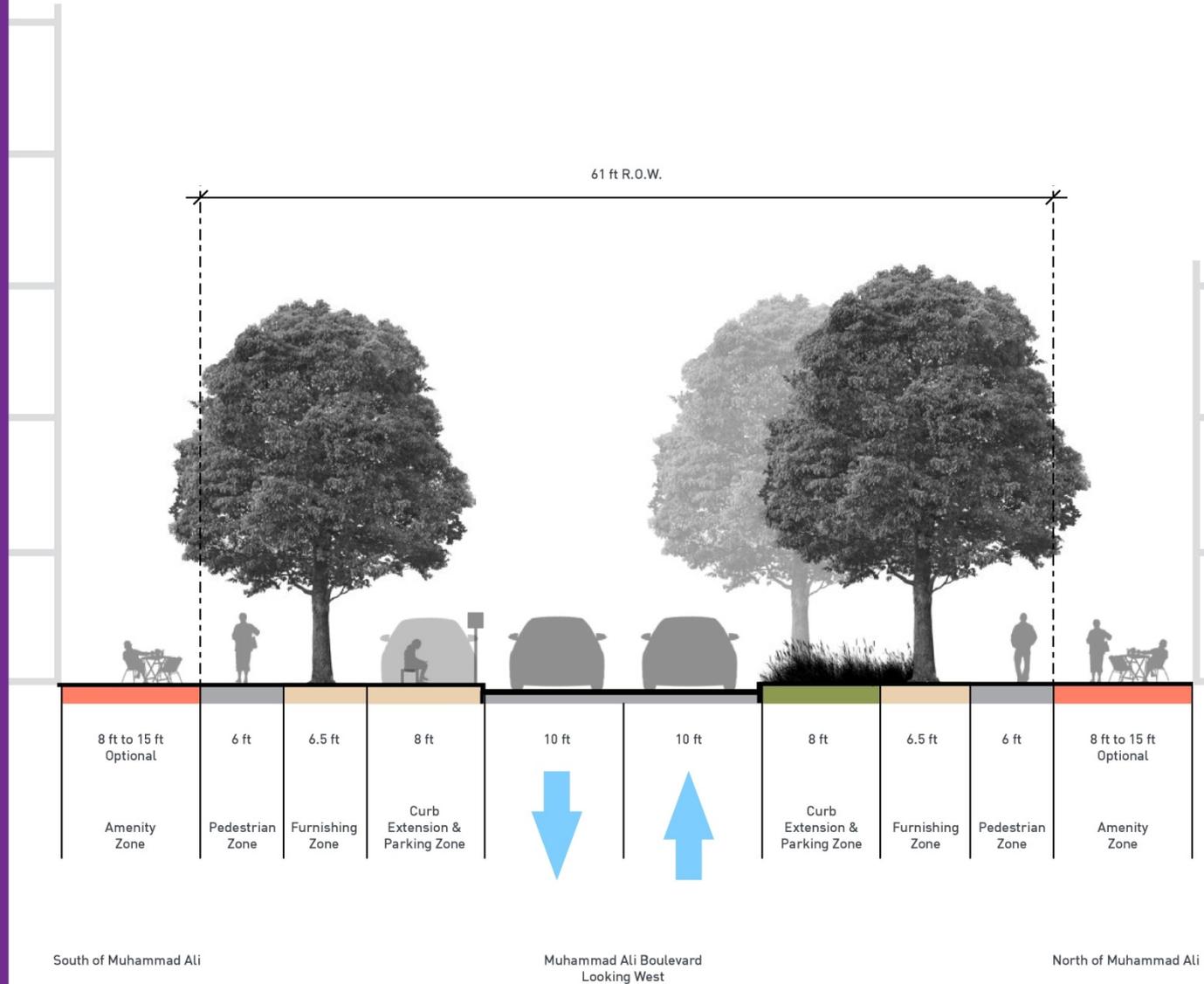
9th St - 14th St

PROPOSED SECTION - TYPICAL

The Walnut District is a residential and commercial corridor that maximizes public spaces for social gatherings, on-street amenities, and display of art/culture. Narrow travel lanes and on-street parking facilities will help maintain slower traffic speeds along Muhammad Ali.

This district is less dense than the Gardens District but more dense than the Heritage District. Buildings within this district will maintain 3 stories to 5 stories. Buildings above 3 stories will "step back" to maintain a pedestrian scale along the street frontage. Buildings will also observe a "zero lot" setback to maintain a consistent street wall unless providing an additional 8 ft to 15 ft of pedestrian amenity space. An example of pedestrian amenity space is on-street cafe seating. Businesses will be transparent on the first floor with large store front windows.

- Curb Extension / Parking Zone**
 Curb extensions in this district offer extra amenity space and are located at street corners, transit stops, and where applicable, possible mid-block crossings.
- Furnishings Zone**
 The furnishings zone is reserved for streetscape furniture and landscape opportunities.
- Pedestrian Zone**
 The pedestrian zone is a concrete path reserved for pedestrians only and shall not be obstructed by any landscape, furniture, or amenity space.
- Amenity Zone**
 Businesses without a zero lot line shall offer additional amenity space at the street level. One example is cafe seating in front of a restaurant.



THE WALNUT DISTRICT - EXISTING



THE WALNUT DISTRICT - PROPOSED

Looking South-West

"Street Wall"

Street Trees

Pedestrian Lighting

Mixed-Use Infill

Interpretive Displays

On Street Parking

Enhanced Landscape

Pedestrian Amenities

Wide Sidewalks

Active Street Front



THE WALNUT DISTRICT - PROPOSED

Looking West



Unique Details to the Walnut District

1) Medium, rectangular pavers in a running bond pattern, perpendicular to the street. Located within curb extensions, furnishing zones, and amenity zones.

2) Pedestrian zone is a concrete sidewalk path.

3) Simple, straight, wood benches in the furnishing zone offer flexible configurations.

4) Public art opportunities are located at street corners/ curb extensions and pocket parks.

5) Street trees in all districts shall be selected from the recommended urban tree list provided in the design guideline portion of this document. Similar tree species should be planted in succession in lines of eight to ten trees to help create a movement along the corridor. Tree species on the recommended urban tree list will vary in mature size and should be determined based on land-uses adjacent to the plantings.

THE WALNUT DISTRICT - PROPOSED

Looking West



THE WALNUT DISTRICT - PROPOSED

Looking West

LCCC











Heritage District

14th St - 22nd St

EXISTING CONDITIONS

The Heritage District falls within the existing Russell Historic District. The area contains several vacant lots that were formerly single family homes as well as a number of established churches and institutions. The intersection of 18th and Ali is an important node made up of a mix of uses that includes the Kentucky Center for African American Heritage. This center is a vital catalyst, attracting hundreds of visitors to the Russell Neighborhood every weekend with large events.

- **No Street Trees**

Street Trees help make pedestrians feel comfortable. They also help create a buffer between the street traffic and where a pedestrian is traveling.

- **Inconsistent Fencing**

Inconsistent fencing along a neighborhood street creates an uninviting experience and inconsistent cohesive feel to the streetscape.

- **Wide Travel Lanes**

Wide travel lanes encourage faster traffic making it uncomfortable for pedestrians traveling beside the street.

- **Front Yard Parking**

Parking in front yards creates a visual impact that disrupts the continuity of the neighborhood.

- **Vehicular Scale Lighting**

Vehicular scale lighting lights the street instead of the sidewalk where people are traveling.



Heritage District

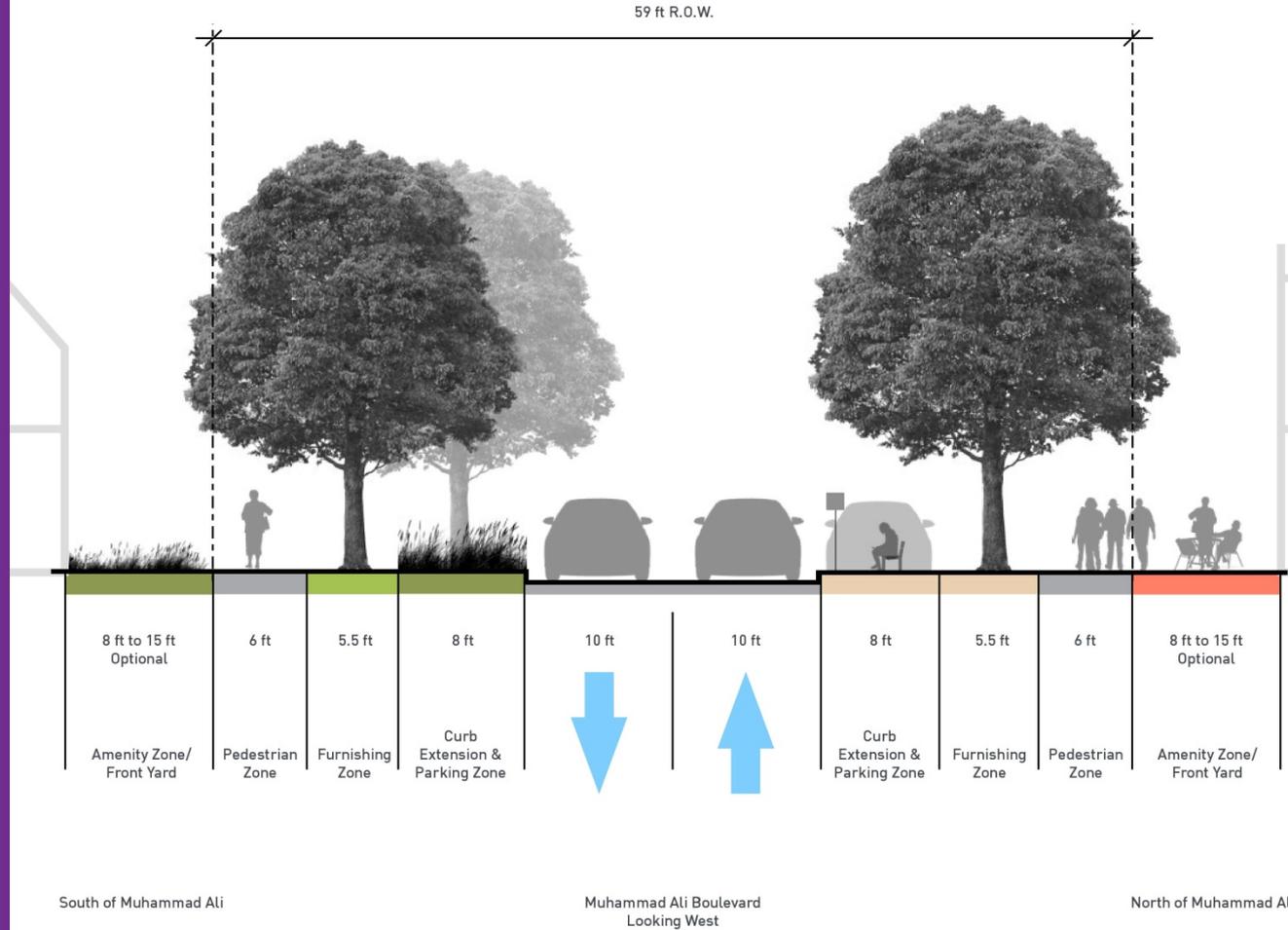
14th St - 22nd St

PROPOSED SECTION - TYPICAL

The Heritage District observes a quieter atmosphere with slower traffic speeds and lower traffic volume. Because of the residential nature of this district, dense landscaping, street trees and pedestrian amenities like seating and bicycle parking make the streetscape acts as an extension of the front yard. Buildings in this district are 1.5 stories to 2.5 stories in height and may have a variable setback of 8 ft to 15 ft.

This district will also focus on street frontage and strategic infill opportunities. Building scales will be compatible with the adjacent properties. Permanent residential parking will be accessed through alley entrances only. Temporary parking will be provided through on-street parking.

- Curb Extension / Parking Zone**
 Where parking is not designed, curb extensions are located at street corners and transit stops and offer additional amenity space.
- Furnishings Zone**
 The furnishings zone is reserved for landscaping opportunities only in this district.
- Pedestrian Zone**
 The pedestrian zone is the sidewalk path where pedestrians travel. This path shall remain clear at all times, uninhibited from obstructions like street furniture, signage, and landscaping. The material for this zone is concrete.
- Amenity Zone**
 The amenity zone in this district is reserved for small cafes and small businesses that offer additional public space.



THE HERITAGE DISTRICT - EXISTING



THE HERITAGE DISTRICT - PROPOSED

Looking West



"Street Wall"

Street Trees

Mixed-Use Infill

On Street Parking

Live-Work

Active Street Front

Transit Access

Consistent Edge

Wide Sidewalks

Pedestrian Amenities

THE HERITAGE DISTRICT - PROPOSED

Looking North-West



Unique Details to the Heritage District

1) Medium size rectangular pavers in a running bond pattern perpendicular to the street. Located within curb extensions.

2) Furnishing zone is reserved for landscape opportunities only.

3) Pedestrian zone is a concrete sidewalk path.

4) Simple wood benches with a back located within curb extensions and around transit access areas.

5) Public art opportunities are located at street corners or within pocket parks.

6) Street trees in all districts shall be selected from the recommended urban tree list provided in the design guideline portion of this document. Similar tree species should be planted in succession

in lines of eight to ten trees to help create a movement along the corridor. Tree species on the recommended urban tree list will vary in mature size and should be determined based on land-uses adjacent to the plantings.

THE HERITAGE DISTRICT - PROPOSED

Looking West



THE HERITAGE DISTRICT - PROPOSED

Licking West



DESIGN GUIDELINES

Site Furnishings

Site furnishings shall not obstruct ADA access.

i. Seating

1. Seating should be integrated in sidewalk planting & furnishings zones, ideally under street trees for shade.
2. Informal seating, such as seat walls or raised planters can be utilized as seating.
3. Seating should be utilized to help formulate social spaces. Where possible seating should be perpendicular to the curb line.
4. Cafe seating - (see Amenity Zones)

ii. Bicycle Racks

1. Bike racks should be located where, with bicycles in rack there is still a 6 ft clear ADA zone.
2. Bicycle racks should be placed in furnishing & planting zone.
3. Bike racks should be 2 ft min. from curb and ideally located perpendicular to the curb line.
4. Bicycle parking should be located near building entrances and high areas of activity, where they will be most utilized.

iii. Trash containers

1. Trash containers should be located near intersection corners and near areas of high activity.
2. Trash containers should be placed every 200 ft to 250 ft or at least 2 per street intersection.

- iv. Signs should not clutter, detract from, or otherwise diminish a street's visual quality.



Heritage District



Garden District



Walnut District

DESIGN GUIDELINES

Curb Extensions

Curb extensions shall be in compliance with ADA standards

i. Corner curb extensions

1. Curb extensions should be used to maximize pedestrian space.
2. Curb extensions should be used on streets-with on-street parking.
3. Curb extensions should be used to increase pedestrian visibility and reduce crossing lengths.
4. Curb extensions can be utilized at transit stop locations to increase space available to shelters and other stop related design features.
5. Curb extensions are opportune spaces for increased landscaping zones, street trees, street seating and should be protected by bollards.

ii. Curb extensions should be utilized to reduce overall crosswalk lengths.



DESIGN GUIDELINES

Street Trees

Street trees shall comply with Chapter 10.2.8 of the Land Development Code requiring street trees along all residentially zoned property and arterial roadways.

i. Planting Design

1. Tree pits should be as large as possible to allow for ample root growth, crown and prevent future sidewalk lifting.
2. Optimal tree pit size is 6 feet by a minimum of 10 feet.
3. Continuous tree pits are preferable.
4. Street trees shall be spaced 30 ft on center wherever possible or appropriate.
5. Species planted under power lines shall be spaced 15 ft on center wherever possible or appropriate.

ii. Species selection

1. Street tree species selection shall be completed in coordination with the Louisville Metro Urban Forestry Department/City Arborist. Tree species selection shall be in compliance with the Preferred Plant List within Chapter 10 of the Land Development Code.
2. Species shall be hardy, low maintenance species and appropriately spaced.
3. Species to be planted under utility lines should have a mature height that is lower than the lines.

- iii. Mature street trees extending into the pedestrian zone shall be limbed up to a height of 8 ft.



DESIGN GUIDELINES

Planters & Plantings

The furnishing & planting zone is between the pedestrian zone and street curb and shall not obstruct ADA access on walkways.

- i. It shall include some or all of the following:
 1. Utility poles and street and signage parking.
 2. Lighting.
 3. Street trees.
 4. Trash containers.
 5. Outdoor seating and benches.
 6. Bicycle parking.
 7. Planters and/or planter beds.
- ii. All landscape shall utilize the 3:8 rule, where no vegetation is allowed between 3 ft and 8 ft from ground level.
- iii. Developments that are set back from the right-of-way shall be further landscaped.
- iv. Landscaping must be provided to cover all street facing exposed foundations.
- v. Streetscape planters and plantings should use native species flowers, grasses and small shrubs. Resources available through the Kentucky Cooperative Extension Service, (<https://extension.ca.uky.edu/>).
- vi. Recommended urban tree list:
 1. Pin Oak
 2. Thornless Honeylocust (Fruitless and thornless)
 3. Gingko (fruitless)
 4. Tree Lilac
 5. Osage Orange (fruitless and thornless)
 6. River Birch



DESIGN GUIDELINES

Lighting

Pedestrian scale lighting shall not obstruct ADA access.

- i. Street lights should be located near the curb line and within the planting & street furnishings zone.
- ii. Street lights and other pedestrian lighting should be coordinated with other streetscape elements.
- iii. Lighting should not be located next to street tree plantings which may impact lighting.
- iv. Lighting should be spaced consistently, appropriate, and on both sides of the street.
- v. Light fixtures should be selected to provide efficient, direct light.
- vi. Be no taller than 14 ft.
- vii. Provide at least 8 ft of clearance.
- viii. Should provide consistent levels of illumination and avoid contrasting pools of light and dark.

Vehicular scale lighting shall:

- i. Be no taller than 24 ft.
- ii. Provide at least 18 ft of vertical clearance.



DESIGN GUIDELINES

Sustainability

- i. Parking.
 1. Negotiate shared parking with adjacent properties.
- ii. Green infrastructure is encouraged within the streetscape and in parking lots where feasible.
 1. Infiltrate runoff through permeable/porous paving.
 2. Convey runoff into bioretention basins.
 3. Slope walkways and driveways into landscape beds.
 4. Slow run-off in bioswales.
 5. Utilize the benefits of rain gardens.
- iii. Integrate prominent and healthy existing trees into the landscape to the extent possible.
- iv. Streetscape planters and plantings should encourage pollinator species and improving Louisville's overall ecological health. Resources on plant materials are available through the Pollinator Partnership (www.pollinator.org).



DESIGN GUIDELINES

Intersections

- i. Crosswalks should be provided at all street intersections along Muhammad Ali Boulevard.
- ii. Crosswalks across minor intersections shall include one or more of the following:
 1. Clear signage and/or crossing aids.
 2. Painted pavement and/or change in pavement material.
 3. Raised crosswalk.
- iii. Crosswalks at major intersections shall include both:
 1. Crossing aids.
 2. Painted pavement or change in pavement material.
- iv. Art Moments are encouraged at most if not all intersections.



DESIGN GUIDELINES

Pocket Parks

Pocket parks are semi-public spaces owned by a private interest. They are for the use of the general public and shall not restrict ADA access.

- i. Mural art is encouraged for blank walls.
- ii. These spaces are used for all of the following, or a combination of:
 1. The display of plants.
 2. The display of art, historical, or cultural pieces.
 3. Social activation spaces.
- iii. Pocket Parks should provide appropriate pedestrian amenities including several of the following:
 1. Educational/informational signage.
 2. Pedestrian lighting.
 3. Outdoor seating.
 4. Bicycle parking.
 5. Trash containers.
 6. Short or low height plantings.
 7. Shade trees.
 8. Public art.

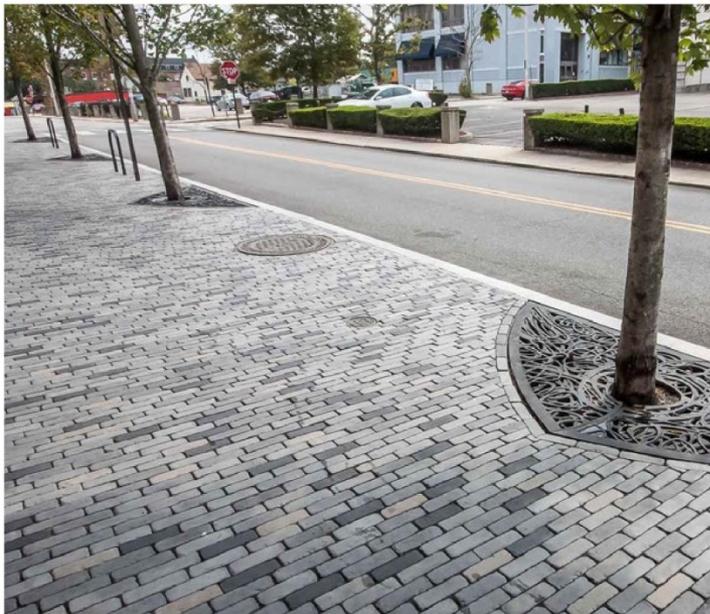


DESIGN GUIDELINES

Hardscape Elements

Hardscape elements shall not obstruct ADA access.

- i. Paving materials that are appropriate include:
 1. Concrete, porous or traditional.
 2. Brick, stone, or other well-maintained pavers.
 3. Asphalt, only for vehicle use.
- ii. Hardscape elements may include:
 1. "Stumbling Blocks" - small, specific pavers denoting moments in history or significant historical sites of former businesses, spaces, or residences.
- iii. Special hardscape elements should be considered within/ around:
 1. Pocket Parks.
 2. Transit stops.
 3. Curb extensions.
 4. Art Moments.
- iv. Hardscape elements may be located within the amenity zone, pedestrian zone, or furniture & plantings zone.



DESIGN GUIDELINES

Public Art

Public art opportunities include, but not limited to:

- i. Sculptural additions to the streetscape in the form of unique:
 1. Bicycle parking.
 2. Transit stops that include shelters.
 3. Fencing or amenity space delineators.
 4. Standalone sculptures.
- ii. Mural walls.
- iii. Interpretive Displays.
- iv. Educational signage.
- v. Monuments.
- vi. Statues.
- vii. Other permanent structures intended for ornament or commemoration on City property.



The BLVD

Order of Magnitude Costs

Below is a summary of the planning level order of magnitude cost estimate for the 1.55 miles of streetscape improvements proposed for the Muhammad Ali Arts and Cultural District. Estimate ranges have been provided for each sub-district as well as the overall District from 5th Street to 22nd Street:

- Gardens District – Approx 0.35 Miles and 4 Blocks - \$4.2M
- Walnut District – Approx 0.56 Miles and 6 blocks - \$5.5M
- Heritage District – Approx 0.64 Miles and 7 blocks - \$2.8M.
- Total length 1.55 miles of streetscape on both sides of the street
- Total estimated range of improvement costs - \$11.3M-\$13.8M

The following is a breakdown of estimated costs by sub-district:

Garden District: Order of Magnitude Engineer's Estimate - Per Block				
Item	Unit	Quantity	Unit Cost	Total
Design and Mitigation Fees				
Design Fees Not including Permit or Bonding Fees (10%)	LS	1	\$ 82,000	\$ 82,000
Sub-Total				\$ 82,000
General Site Construction				
Construction Survey	LS	1	\$ 5,000.00	\$ 5,000
Sub-Total				\$ 5,000
Site Improvements				
Mobilization (8%)	LS	1	\$ 67,900.00	\$ 67,900
Demolition	LS	1	\$ 3,000.00	\$ 3,000
Erosion Control	LF	1,000	\$ 3.50	\$ 3,500
Storm Drainage Relocation	LS	1	\$ 10,000.00	\$ 10,000
Concrete Sidewalks	SF	7,460	\$ 7.50	\$ 55,950
Planting Verges	SF	2,490	\$ 1.50	\$ 3,735
Street Lights	EA	12	\$ 4,000.00	\$ 48,000
Speciality Pavers	SF	15,200	\$ 30.00	\$ 456,000
Site Furnishings	EA	10	\$ 3,500.00	\$ 35,000
Transit Shelters	EA	0.4	\$ 40,000.00	\$ 16,000
Conc. Curb/Gutter	LF	1,000	\$ 30.00	\$ 30,000
Road Re-Surfacing	SF	11,880	\$ 4.00	\$ 47,520
Striping	LF	430	\$ 0.60	\$ 258
Sub-Total				\$ 776,863
Landscaping				
Small Trees	EA	8	\$ 250.00	\$ 2,000
Large Trees	EA	18	\$ 500.00	\$ 9,000
Sub-Total				\$ 11,000
Sub-Total				\$ 874,863
Contingency			20%	\$ 174,973
Grand Total				\$ 1,049,836

Notes:

Since we do not have control over the cost of labor, materials or equipment, or over the Contractor's method of costing in the marketplace, the opinion of probable cost as herein stated is made on the basis of our experience and qualifications and represent our best judgement as a design professional familiar with the construction industry. However, we cannot guarantee that bids or the construction cost will not vary from these probable cost opinions. If the Owner desires greater assurance of the cost of construction, it is recommended he employ an independent cost estimator.

Exclusions:

- Utility or service or infrastructure (electric, natural gas, telecom, etc.) including relocation
- Sanitary sewer pump stations or force main
- Amenity site or features
- Irrigation
- Rock excavation or trenching
- Offsite roadway or utility improvements
- Unsuitable/Contaminated Soil Undercut and/or Haul Off
- Land / Easement Acquisitions

District Total - 4 Blocks (.35 Miles)
\$ 4,199,342

Walnut District: Muhammad Ali Blvd Order of Magnitude Engineer's Estimate - Per Block				
Item	Unit	Quantity	Unit Cost	Total
Design and Mitigation Fees				
Design Fees Not including Permit or Bonding Fees (10%)	LS	1	\$ 76,700	\$ 76,700
Sub-Total				\$ 76,700
General Site Construction				
Construction Survey	LS	1	\$ 5,000.00	\$ 5,000
Sub-Total				\$ 5,000
Site Improvements				
Mobilization (8%)	LS	1	\$ 62,000.00	\$ 62,000
Demolition	LS	1	\$ 3,500.00	\$ 3,500
Erosion Control	LF	980	\$ 3.50	\$ 3,430
Storm Drainage Relocation	LS	1	\$ 10,000.00	\$ 10,000
Concrete Sidewalks	SF	5,280	\$ 7.50	\$ 39,600
Planting Verges	SF	5,850	\$ 1.50	\$ 8,775
Street Lights	EA	12	\$ 4,000.00	\$ 48,000
Speciality Pavers	SF	12,050	\$ 30.00	\$ 361,500
Site Furnishings	EA	10	\$ 3,500.00	\$ 35,000
Transit Shelters	EA	0.3	\$ 40,000.00	\$ 13,333
Conc. Curb/Gutter	LF	980	\$ 30.00	\$ 29,400
Road Re-Surfacing	SF	13,400	\$ 4.00	\$ 53,600
Striping	LF	670	\$ 0.60	\$ 402
Sub-Total				\$ 668,540
Landscaping				
Small Trees	EA	8	\$ 250.00	\$ 2,000
Large Trees	EA	24	\$ 500.00	\$ 12,000
Sub-Total				\$ 14,000
Sub-Total				\$ 764,240
Contingency			20%	\$ 152,848
Grand Total				\$ 917,088

Notes:

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- Sanitary sewer pump stations or force main
- Amenity site or features
- Irrigation
- Rock excavation or trenching
- Offsite roadway or utility improvements
- Unsuitable/Contaminated Soil Undercut and/or Haul Off
- Land / Easement Acquisitions

District Total - 6 Blocks (.56 Miles)
\$ 5,502,530

Heritage District: Muhammad Ali Blvd Order of Magnitude Engineer's Estimate - Per Block				
Item	Unit	Quantity	Unit Cost	Total
Design and Mitigation Fees				
Design Fees Not including Permit or Bonding Fees (10%)	LS	1	\$ 32,900	\$ 32,900
Sub-Total				\$ 32,900
General Site Construction				
Construction Survey	LS	1	\$ 5,000.00	\$ 5,000
Sub-Total				\$ 5,000
Site Improvements				
Mobilization (8%)	LS	1	\$ 26,000.00	\$ 26,000
Demolition	LS	1	\$ 3,000.00	\$ 3,000
Erosion Control	LF	864	\$ 3.50	\$ 3,024
Storm Drainage Relocation	LS	1	\$ 10,000.00	\$ 10,000
Concrete Sidewalks	SF	5,280	\$ 7.50	\$ 39,600
Planting Verges	SF	4,645	\$ 1.50	\$ 6,968
Street Lights	EA	5	\$ 4,000.00	\$ 20,000
Speciality Pavers	SF	1,450	\$ 30.00	\$ 43,500
Site Furnishings	EA	10	\$ 3,500.00	\$ 35,000
Transit Shelters	EA	2/7	\$ 40,000.00	\$ 11,429
Conc. Curb/Gutter	SF	864	\$ 30.00	\$ 25,920
Road Re-Surfacing	SF	13,110	\$ 4.00	\$ 52,440
Striping	LF	818	\$ 0.60	\$ 491
Sub-Total				\$ 277,371
Landscaping				
Small Trees	EA	8	\$ 250.00	\$ 2,000
Large Trees	EA	24	\$ 500.00	\$ 12,000
Sub-Total				\$ 14,000
Sub-Total				\$ 329,271
Contingency			20%	\$ 65,854
Grand Total				\$ 395,125

Notes:

Since we do not have control over the cost of labor, materials or equipment, or over the Contractor's method of costing in the marketplace, the opinion of probable cost as herein stated is made on the basis of our experience and qualifications and represent our best judgement as a design professional familiar with the construction industry. However, we cannot guarantee that bids or the construction cost will not vary from these probable cost opinions. If the Owner desires greater assurance of the cost of construction, it is recommended he employ an independent cost estimator.

Exclusions:

- Utility or service or infrastructure (electric, natural gas, telecom, etc.) including relocation
- Sanitary sewer pump stations or force main
- Amenity site or features
- Irrigation
- Rock excavation or trenching
- Offsite roadway or utility improvements
- Unsuitable/Contaminated Soil Undercut and/or Haul Off
- Land / Easement Acquisitions

District Total - 7 Blocks (.64 Miles)
\$ 2,765,875

Total Corridor Costs
\$ 12,467,748