



Historic Landmarks and Preservation Districts Commission

Report to the Committee

To: Clifton Architectural Review Committee
Thru: Cynthia Elmore, Historic Preservation Officer *CE*
From: Anthony Schneider, Historic Preservation Specialist
Date: May 14, 2019

Case No: 19COA1069
Classification: Committee Review

GENERAL INFORMATION

Property Address: 1848 Frankfort Avenue

Applicant: Eric Claypool
EC Design
8801 Hermitage Court
Louisville, KY 40242
502-742-5200
502-619-2640
ericclaypool@att.net

Owner: Todd Troy
1848 Frankfort Avenue
Louisville, KY 40206
502-802-2659
trtroy14@gmail.com

Estimated Project Cost: \$50,000.00

Description of proposed exterior alteration:

The applicant is requesting to construct a new, two-story, four-car carriage house at the rear of the property with access to the garages from Stoll Avenue. The structure will have a dwelling unit above the parking garages. The structure will have a hip roof, 4" vinyl siding, and vinyl windows with a three-over-one configuration. The structure will also have individual garage door openings.

Communications with Applicant, Completion of Application

The application was received on March 14, 2019 and was considered complete and requiring committee review on March 27, 2019. Staff met with the property owner and the applicant on March 27, 2019 to discuss the guidelines and

recommended changes. Staff also informed the applicant that they would need to pursue discretionary variances and waivers from the Board of Zoning Adjustment and the Planning Commission.

This case is scheduled to be heard by the Clifton Architectural Review Committee on Wednesday May 22, 2019 at 6:30pm in Conference Room 101, 444 S 5th Street Metro Development Center.

FINDINGS

Guidelines

The following design review guidelines, approved for the Clifton Preservation District, are applicable to the proposed exterior alterations: **Garage** and **New Construction-Residential**. The report of the Commission Staff's findings of fact and conclusions with respect to these guidelines is attached to this report.

The following additional findings are incorporated in this report:

Site Context/ Background

The R-7 zoned property within a Traditional Neighborhood Form District is located on the southwest corner of Stoll Avenue and Frankfort Avenue. The structure is a 2 ½ story frame home that currently functions as apartments. The structure has a Queen Anne form, but many of the ornamental details have been covered or removed. A defining feature is the three-story turret with an octagon design and complimentary roof form.

Staff did not find previous COA's issued for this site.

Conclusions

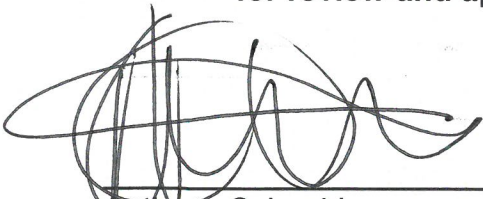
The project generally meets the Clifton design guidelines for **Garage** and **New Construction-Residential**. The proposed structure is rather large for an accessory structure but remains subordinate to the massing and scale of the primary structure on the lot. The proposed carriage house will have four individual garage doors and two person-doors on the yard side of the structure. The applicant worked with staff on selecting a three-over-one window configuration and for the inclusion of a water-table board along the foundation for a more appropriate siding to trim ratio. The proposed cladding is a 4" smooth-faced vinyl siding with a traditional lap. All of the design elements are consistent with the requirements of Guidelines **NCR-5**, **NCR-7**, **NCR-12**, and **NCR-13**. However, staff finds that the garage only partially meets Guideline **G3** as the garage is situated at the alley, but access is gained from Stoll Avenue—a secondary street to Frankfort Avenue. Staff finds that the access from Stoll Avenue to be consistent with Guideline **G8** making the proposed orientation justifiable. Additionally, the Sanborn Map 1928-1941 shows a two- and one-story accessory structure at the rear of the lot and encompassing a similar footprint to the structure proposed. In the research process, staff found that there are four potentially historic curb cuts in the limestone curbing along Stoll Avenue which could indicate the existence of a historic accessory structure with access to Stoll Avenue.

It is important to note that in the initial review with the applicant, staff found that the applicant will need to acquire discretionary waivers and variances from the Board of Zoning Adjustment and/or the Planning Commission prior to construction. These will need to be obtained for the proposal to be in accordance with design Guideline **NCR-1**.

DECISION

On the basis of the information furnished by the applicant, staff recommends that the application for a Certificate of Appropriateness be **approved with the following conditions:**

1. The applicant shall obtain all necessary building permits.
2. There shall be trim around all openings and corner boards on all corners.
3. New siding shall be smooth-face, traditional lap siding with a 3" or 4" exposure.
4. A water-table board and drip cap shall be installed around the foundation.
5. Applicant shall repair or replace any damaged concrete elements with historic concrete mix.
6. Drive apron and all new concrete walks shall be poured using historic concrete mix.
7. Applicant shall make necessary provisions for the screening and storage of trash receptacles.
8. Storm water management provisions shall be incorporated into the structure as to reduce the impact on nearby historic resources.
9. Applicant shall submit proposed exterior lighting to staff prior to installation.
10. All exterior lighting shall be directed downward as to minimize the impact on adjacent properties.
11. Should the variance & waiver requests be denied or modified, applicant shall work with staff on the rereview of the proposed new construction.
12. If design changes are necessary, the applicant shall contact staff for review and approval.



Anthony Schneider
Historic Preservation Specialist

8/15/19

Date

Attached Documents / Information

1. Staff Guideline Checklist
2. Louisville Sanborn Map 1928-1941

Garage

Clifton Design Guideline Checklist

- + Meets Guidelines
- NA Not Applicable
- Does Not Meet Guidelines
- NSI Not Sufficient Information
- +/- Meets Guidelines with Conditions

	Guideline	Finding	Comment
G1	Contributing secondary structures should be preserved. However, when demolition is being requested to make way for a new secondary building, then Landmarks staff and/or the ARC will evaluate and review the demolition permit request based on the structure's integrity, historical character and materials, functionality, and security concerns. All structures in the district will be identified as either contributing or non-contributing at the time of application. See the Demolition guidelines for more details.	NSI	
G2	New garages or other secondary structures should be designed so they complement the scale, mass, roof form, setback, and materials of adjacent secondary structures. They should also be subordinate to the primary structure.	+	New carriage house/garage structure will be 2 stories in height and will have a hip roof. The structure is smaller than the existing, even though it is large for the district.
G3	New garages should be sited adjacent to an alley where present. Review the garage prototype illustration that identifies styles appropriate to preservation districts when planning a garage construction project.	+/-	This house is sited toward Stoll avenue rather than an alley. There are four existing curb cuts in the concrete and limestone off Stoll Avenue which is why staff was inclined to support. See attached Sanborn map.
G4	When no alley exists, garages should be sited at the rear of the property behind the main house. Landscape screening is encouraged along the driveway.	+	
G5	Single garage doors should be used rather than expansive double or triple doors.	+	
G6	The roofline of a new garage should be oriented so it is parallel with the main house or follows the predominant pattern of existing secondary structures when a pattern exists.	+	
G7	Roof pitch should be no less than one in six. The roof form of the garage should match the roof form of the main house when it is a character-defining feature.	+	
G8	New garages should be designed so access to off-street parking is off alleys or secondary streets wherever possible.	+	Stoll is secondary to Frankfort Avenue.
G9	New garages should be located at the rear of the property, should define and enclose the rear yard, and should be aligned with adjacent secondary structures.	+	
G10	The garage design should be simple and rectangular in shape. Ell-shaped floor plans, slightly-projecting bays, and cantilevered second floors are also permitted.	+	
G11	New garage walls should be constructed with any of these materials (1) Horizontal siding to match existing exposure of the primary structure (normally 3" or 4" exposure), (2) corner boards and trim around openings, (3) board and batten siding, (4) brick, (5) stucco over frame or concrete block, (6) painted concrete block with parged or flush joint finish, (7) cast stone, molded concrete block, or (8) wood, aluminum or vinyl siding, or fiber cement siding or board to match existing	+	See Conditions.

	Guideline	Finding	Comment
	exposure of the primary structure. Do not use these materials: T-111, exposed uncoated concrete block, or painted concrete block unless parged or skim coated first.		
G12	Approvable roof designs include simple gable roofs (6-in-12 minimum slope), hipped, shed, and flat roofs with parapets, intersecting gables, overhanging eaves, and gable end-vents. Not approvable are low-pitched gable roofs (less than 6-in-12 slope), flush eaves, and roofs without gutters.	+	
G13	Asphalt, fiberglass, wood, tile, metal, slate or synthetic shingles are recommended roof materials. Half-round or ogee gutters, gable-end elements, and solar collectors are approvable. Do not use membrane or roll roofing on sloped roofs with 3-in-12 pitch or greater. See Roofing guidelines for additional details.	+	
G14	Single-car garage doors or openings are preferred. Double- or triple-wide doors which convey the appearance of 2 or 3 single doors may be approved. Flush garage doors which accentuate the large size of the opening are prohibited.	+	Building will have single doors.
G15	Garage window openings should be used that visually break up the wall's surface and may be placed at higher elevations for security. Security grills may be installed on the inside face of the windows.	+	

New Construction - Residential

Clifton Design Guideline Checklist

- + Meets Guidelines
- NA Not Applicable
- Does Not Meet Guidelines
- NSI Not Sufficient Information
- +/- Meets Guidelines with Conditions

	Guideline	Finding	Comment
NCR1	New construction designs should conform to all applicable regulations including the Land Development Code, Zoning District Regulations, Building, and Fire and Safety codes, MSD, and any other regulatory agency. All new construction architectural designs will be reviewed by the Clifton ARC.	+	New carriage house structure will need variances and waivers.
NCR2	No structure should be demolished to make way for new or large-scale construction. All structures in the district will be identified as either contributing or non-contributing at time of application. The Landmarks staff and ARC will evaluate and review all demolition permit requests. See the Demolition guidelines for more details.	+	
NCR3	Building height, scale, massing, volume, directional emphasis, and setback should reflect the architectural context established by surrounding structures.	+	
NCR4	The scale of new construction should not conflict with the historic character of the district.	+	
NCR5	Building materials and design elements in new construction design should be sympathetic with surrounding historic buildings in the district. Materials should be of a complementary color, size, texture, scale, and level of craftsmanship.	+	New building materials are in-kind and modern in composition. Windows and siding will be of vinyl composition while retaining the traditional form.
NCR6	Creative design is encouraged. Examples of materials to avoid include: ornamental pierced concrete masonry screens and walls, "antiqued" brick, wrought-iron porch columns, exterior carpeting, jalousie windows, glass block, picture windows, unfinished wood, and asphalt siding.	+	

	Guideline	Finding	Comment
	Chain-link fences should not be installed where visually incompatible.		
NCR7	New construction design should reflect and reinforce the human scale of the neighborhood, which is a character-defining feature of the preservation district.	+	Structure is two-stories and within scale of surrounding buildings. There are few structures that face Stoll Ave. and this will help maintain and restore a street wall along the corridor.
NCR8	Important public views and vistas should not be disrupted by new construction design. See the Cultural Landscape guidelines for more details.	+	
NCR9	Existing spatial patterns created by circulation routes, fences, walls, lawns, and allees of trees, should be reinforced in new construction design.	+	Location will be where an accessory structure once existed.
NCR10	The spatial organization established by surrounding buildings should be reinforced in infill construction design. The character of historic streetscapes relies heavily on the visual continuity established by the repetition of similarly designed façades.	+	
NCR11	The façade's organization should closely relate to surrounding buildings in infill construction design. Cornice lines and columns are other important character-defining façade elements. Imitating an historic style or period of architecture in new construction is not recommended.	+	
NCR12	A new building's mass should have a similar sense of lightness or weight as surrounding historic structures. Mass is determined by the proportion of solid surfaces (walls) to voids (window and door openings).	+	New building will have a number of windows and openings to break up the massing.
NCR13	Window patterns should be sympathetic with those of surrounding buildings. Compatible frame dimensions, proportion, panel and light, and muntin configurations are encouraged. Historic window proportions are generally two-and-one half (height) by one (width).	+	Staff recommended the use of a 3-over-1 design rather than the originally proposed colonial style.
NCR14	Front door design should be sympathetic to the door patterns of surrounding buildings in new construction design. Use of comparable frame dimensions, proportion, and panel and light configuration is encouraged.	NSI	
NCR15	The orientation of the main entrance should be the same as the majority of other buildings on the street in new construction design.	NA	
NCR16	Paved walks should be installed between public sidewalks and front entrances where this is a character-defining feature on the street.	+	
NCR17	Handicapped access ramps should be located on secondary elevations (side or rear) wherever possible. If the only option is to install the ramp on the street address façade, it should be installed in a manner that does not damage historic fabric and is as unobtrusive as possible. Removable or portable ramps may also be used.	NA	
NCR18	Infill construction design should be compatible with the average height and width of surrounding buildings.	NA	
NCR19	Horizontal elements such as band boards, brick coursing, window sills or lintels in new construction design should be within 10 percent of adjacent historic construction where the similar height of the horizontal elements is relatively consistent, and a character-defining feature.	+	
NCR20	The historic rhythm of the streetscape should be maintained.	+	
NCR21	Historic building setback patterns should be maintained. To maintain the continuity of the streetscape, front setbacks for new construction should either match that of adjacent buildings where all share the same setback or be within 20	+	

	Guideline	Finding	Comment
	percent of neighboring structures in areas with varied setbacks.		
NCR22	Roofs of new buildings should relate to neighboring historic structures in pitch, complexity, and visual appearance of materials.	+	Shallow hip roof design.
NCR23	Rooflines for infill construction design should follow the precedent set by adjacent buildings. Where the predominant form is flat, built-up roofs are preferred. Where the predominant form is complex and steeply pitched, that is preferred. In blocks characterized by shallow-pitched roofs and pronounced overhangs with exposed rafters, these elements should be incorporated.	NA	
NCR24	The orientation of the main roof form in new construction design should be parallel with the majority of other roofs on the street where roof forms are relatively consistent and a character-defining feature.	NA	
NCR25	The existing cornice line on each block should be emphasized in new construction design where this is a character-defining feature.	NA	
NCR26	Rooftops should remain uncluttered and mechanical systems should be obscured from public view in new construction design.	+	
NCR27	Trash receptacles should be screened from public view with a four-sided enclosure.	+	See Conditions.
NCR28	Exterior sheathing should be compatible with surrounding historic buildings. Painted wood siding or fiber cement board is preferred. Vinyl siding may be used for new construction on streets where the predominant historic construction material is wood. See Siding and Trim guidelines for additional details.	+	
NCR29	Masonry types and mortars should be compatible with surrounding buildings. Red brick is the most common masonry material found in the district. See Masonry guidelines for additional details.	NA	
NCR30	Stone or cast-stone sills and lintels should be incorporated into new construction design on streets where these elements are character-defining features.	+	This is an accessory structure and materials are subordinate to that of the principle.
NCR31	Raised masonry foundations which are compatible in proportion and height with surrounding buildings should be used. Foundation materials may be of a warm-toned poured concrete or stuccoed concrete block that has a uniform, textured appearance.	+	
NCR32	New front porches should be built on streets where they are a predominant character-defining feature, and are allowed on other streets, and should be compatible with the form, scale, and detailing of surrounding buildings. New columns should consist of a base, shaft, and capital, and convey the appearance of actually holding up the porch roof.	NA	
NCR33	Porches on newly constructed buildings should be designed so the floor is even with or a maximum of one step below the corresponding floor of the house, the ceiling is even with that of adjacent rooms, the floor is at least 6' deep, the rhythm of the porch bays matches the façade's pattern of solids and voids, and the porch fascia board matches the height of the window head.	NA	
NCR34	Storm-water management systems in new construction design and water runoff should not adversely impact nearby historic resources.	+	See Conditions.

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